

**To:** Loch Lomond & The Trossachs National Park Authority  
**From:** Peter Brett Associates  
**Date:** 28/03/19  
**Subject:** West Riverside Parking Strategy

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We are in receipt of a request from the Planning Authority to provide a response to the following comments in relation to traffic and parking based on the West Riverside & Woodbank House, Balloch, Transport Assessment:

### ***Traffic/Parking***

***Neither West Dunbartonshire Roads or Transport Scotland have raised any issues in relation to the proposals. We have met with representatives from WDC Roads and requested that they further confirm that the transport assessment is robust based on the proposed parameters and that the parking proposed (in relation to the proposed parameters) meets their standards. I will let you know when this further advice is received.***

***Paragraph 3.2.25 of the Planning Statement confirms that a total of 330 spaces will be provided across the site (excluding Woodbank House) – this is 97 net additional spaces discounting the existing 233 spaces that currently exist at Balloch Pier and the Old Station. Please confirm if the proposed provision includes the 44 displaced Park and Ride Spaces requested by WDC or consider the potential impacts of the loss of existing parking associated with the Slipway or Maid of the Loch? Can you please provide a parking plan showing the numbers of parking spaces proposed in each area and distribution across the site (to ensure that a sufficient number of parking spaces are provided in the locations it will be required to service the uses). Please also demonstrate that the areas shown for parking have capacity for the numbers proposed (to ensure we can be satisfied that no additional parking within the site will be required).***

***Please give consideration and respond to the comments contained within the consultation response received from ScotRail which can be viewed on the on-line file.***

### **Applicant's Response**

- 1.1.1 This note builds on the content included in the Transport Assessment to respond to the queries above and confirms the level of parking provision and the proposed parking management strategy for the site, and will cover:
- Relevant Transport Assessment content;
  - Parking standards;
  - Proposed parking strategy; and
  - Response to Abellio ScotRail Consultation Response.

### **Relevant Transport Assessment Content**

- 1.1.2 The scope and approach to the Transport Assessment (TA) was agreed with West Dunbartonshire Council (WDC) Roads Department through a series of meetings and direct correspondence as evidenced in full in Appendix A of the TA. It was agreed through scoping discussions that the parking standards to be applied to the development site, be in accordance with the SCOTS National Roads Development Guidelines (NRDG).
- 1.1.3 The TA was cognisant that as the development proposals further evolve, there may be subsequent variation in the development proposals and associated parking provision. This

should always seek to reflect a balanced level of supply to assist with the management of parking demand to ensure the provision remains appropriate to encouraging the uptake of sustainable modes of travel, particularly for short trips, to reduce the health and environmental impacts of unnecessary car use.

## Parking Standards

- 1.1.4 Table 6.1 as included on page 20 of the TA, set out the proposed quantum of development and the relevant proposed NRDG parking standards to be applied to the land use. Whilst the proposed parking standards have not changed, the interim change to the quantum of proposed residential and accommodation development on the Woodbank House site is noted. This is reflected in a revision to Table 6.1 NRDG Parking Standards as re-presented below:

Table 6.1 - NRDG Parking Standards

Development Use	Quantum	Proposed Standard	Required Provision
Parking to be provided in proximity to Station Square (Pier Road New)			
Brewery incl. pub	300sqm	10 spaces/ 100sqm GFA	30 spaces
Restaurant	150sqm	1 space / 5sqm GFA	30 spaces
Youth hostel	32 beds	1 space/ 4 staff plus customer parking	8 spaces
WDC Park & Ride Requirement	-	-	44 spaces
			<b><u>112 spaces</u></b>
Parking to be provided remote from accommodation (LLS Overspill Parking Area)			
Woodland Lodges (Riverfront)	43	1.5 spaces per lodge <sup>1</sup>	65 spaces
Woodland Lodges (Drumkinnon)	32	1.5 spaces per lodge	48 spaces
			<b><u>113 spaces</u></b>
Parking to be provided at the Pierhead Area			
Apart Hotel & Rest.	60 beds	1 space/ 2.5 beds	24 spaces
Water Park	600sqm	10 spaces/ 100sqm pool area	60 spaces
Iconic Visitor Attraction	TBC	Subject to future planning	-
			<b><u>84 spaces</u></b>
Parking to be provided within new Staff & Service Area			
Staff & Service Area	900sqm	1 space/ 20sqm	45 spaces
			<b><u>45 Spaces</u></b>
Sub-Total at West Riverside			<b><u>354 spaces</u></b>

<sup>1</sup> Bespoke parking figure for all woodland accommodation based on provision of a mix of 1, 2 or 3 bedroom lodges.

Development Use	Quantum	Proposed Standard	Required Provision
Parking to be provided within Woodbank House Site			
Residential units (private)	21	1 or 2 spaces/ dwelling	35 spaces
Woodland Lodges	35	2 spaces/ lodge	70 spaces
Woodland Bothies	15	1.5 spaces/ 1 bed	23 spaces
Holiday Apartments	6	1 space/ bed	11 spaces
			<b><u>139 spaces</u></b>
Sub-Total at Woodbank House Site			<b><u>139 spaces</u></b>
Total Parking Requirement Across Full Application Site			<b><u>493 spaces</u></b>

1.1.5 The following should be noted:

- The parking provision identified above includes WDC's Roads Department requirements for 44 spaces to be retained within the site to support a Park and Ride facility for Balloch Railway Station;
- The parking provision for the land uses at the Station Square and Pierhead Area considers each development land use as standalone and assumes no cross-over between the proposed development uses. In effect, it is assumed that each land use will occupy its parking requirement at the same time and there is no dual use of spaces. As such, our approach to parking provision is robust to demonstrate that sufficient numbers of spaces can be provided at the relevant locations. In the instance of Station Square, development associated parking plus the Park and Ride site, there is a shortfall of three spaces. However, the demand for parking for different land uses will vary throughout the day and the proposed provision of 109 spaces is considered ample to cater for the development associated parking demand. There is a recognition that whilst parking demand must be catered for, over provision is equally unacceptable in the overarching effort to encourage uptake of sustainable travel modes over the private car, wherever practicable;
- Our approach to the TA for both trip generation and parking provision assumes that there is no cross-over between the proposed and/ or existing Loch Lomond Shores (and associated) land uses. Discrete parking provision has therefore not been proposed for the woodland visitor attractions, given:
  - the difficulty in quantifying what proportion of all other visitors to the new development(s) will use the attractions and therefore be on site already;
  - the difficulty in quantifying what proportion of all other visitors to the existing LLS will use the attractions and therefore be on site already;
  - the target age range for the users of the woodland visitor attractions will require to travel with an adult, and it is considered exceptionally unlikely that any one 'visit' by a family will not include a 'visit' to an existing amenity at LLS or be part of the 'visiting' cohort associated with the new development; and
  - Evidence suggests that there is existing ample provision for parking within the existing LLS general car park, and the overspill parking area can be used for those unique occasions when parking demand increases due to events and/ or good weather.
- Paragraph 6.4.5 of the TA noted, "*As the detail of the proposals progress, and dialogue with the other Loch Lomond Shores proprietors continues, it is anticipated that parking locations will be reconfigured to allow effective, efficient and sustainable vehicle and access*

operations across the shared-sites.” Since then, Scottish Enterprise have confirmed, in response to the submission made by Iceni Projects on behalf of Kemble Business Park LLP that:

*"The principle of utilising some existing car parking spaces was agreed verbally at the January 2018 Proprietors Meeting prior to the application being submitted. This in principle agreement will require all parties to enter into and agree a commercial position on a how this would work in practice. At the same meeting, it had been agreed that further negotiations regarding parking would take place following the PPiP determination and prior to the creation of any detailed plans."*

- With respect to the proposed parking provision as part of the PPiP application, it must be recognised that as some elements of the design progress through the planning stages, there may be some revisions to the development scale which would result in an adjustment of the parking provision. At this stage, the revised Table 6.1, reflecting the current development proposals, and the parking strategy appended to this correspondence, demonstrate that the appropriate level of parking provision can be comfortably accommodated within the site extents.

### Proposed Parking Strategy

- 1.1.6 The level of parking provision for the proposed land uses and their proposed locations, through provision of a parking strategy (as appended to this note) demonstrates the proposed parking spaces and distribution across the site.
- 1.1.7 Appendix 1 includes a graphical representation of the West Riverside Proposed Parking Strategy and the Woodbank Site Parking Strategy.

### Response to ScotRail Consultation Response

- 1.1.8 The TA noted that discussions had been held with ScotRail Abellio<sup>2</sup> prior to submission of the West Riverside planning application. Initial discussions agreed in principle the mutual and wider benefits of promoting access to the development site by rail and had identified some potential viable interventions. Thereafter, we had sought further discussions with ScotRail Abellio to advance the content and detail of those principles and interventions, such that they could be more clearly articulated for the purposes of planning. A response was not available at the time of going to planning, therefore, we very much welcome ScotRail Abellio's commitment throughout their 'Assessment Proforma' for positive dialogue and communication to progress matters further.
- 1.1.9 The content of the Scotrail Planning Application Review and Assessment Pro Forma is both helpful and insightful and will play a key role in the advancement of development and design principles for the site. We welcome the points made throughout and will ensure these are embraced when the development advances to the detailed design stages. We note the key headlines in the Proforma and provide summary commentary on these below:
  - **Detailed Response** – we would welcome provision of the Balloch Station Travel Plan;
  - **Institutional Alignment** – ongoing positive dialogue and communication will be key, particularly as the development proposals advance, to ensure greater transport integration;
  - **Integration of Proposed Active Travel Hubs** – we recognise the need to advance the measures to normalise sustainable and inclusive access and this is included in the TA as an Outline Travel Plan. This makes best use of the information and design proposals as available at this stage in the project's development. Going forward this will be refined in

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<sup>2</sup> Referenced in Paragraph 5.7.1 under Access Opportunities & Constraints, Paragraph 6.3.2 under Public Transport (Rail) and under Paragraph 8.5.3 under Public Transport Initiatives.

conjunction with input and dialogue from a variety of stakeholders. It will evolve to ensure a comprehensive, and fit for purpose Detailed Travel Plan (commensurate with the detailed planning stage) is prepared to support the planning, delivery and operational stages of the development;

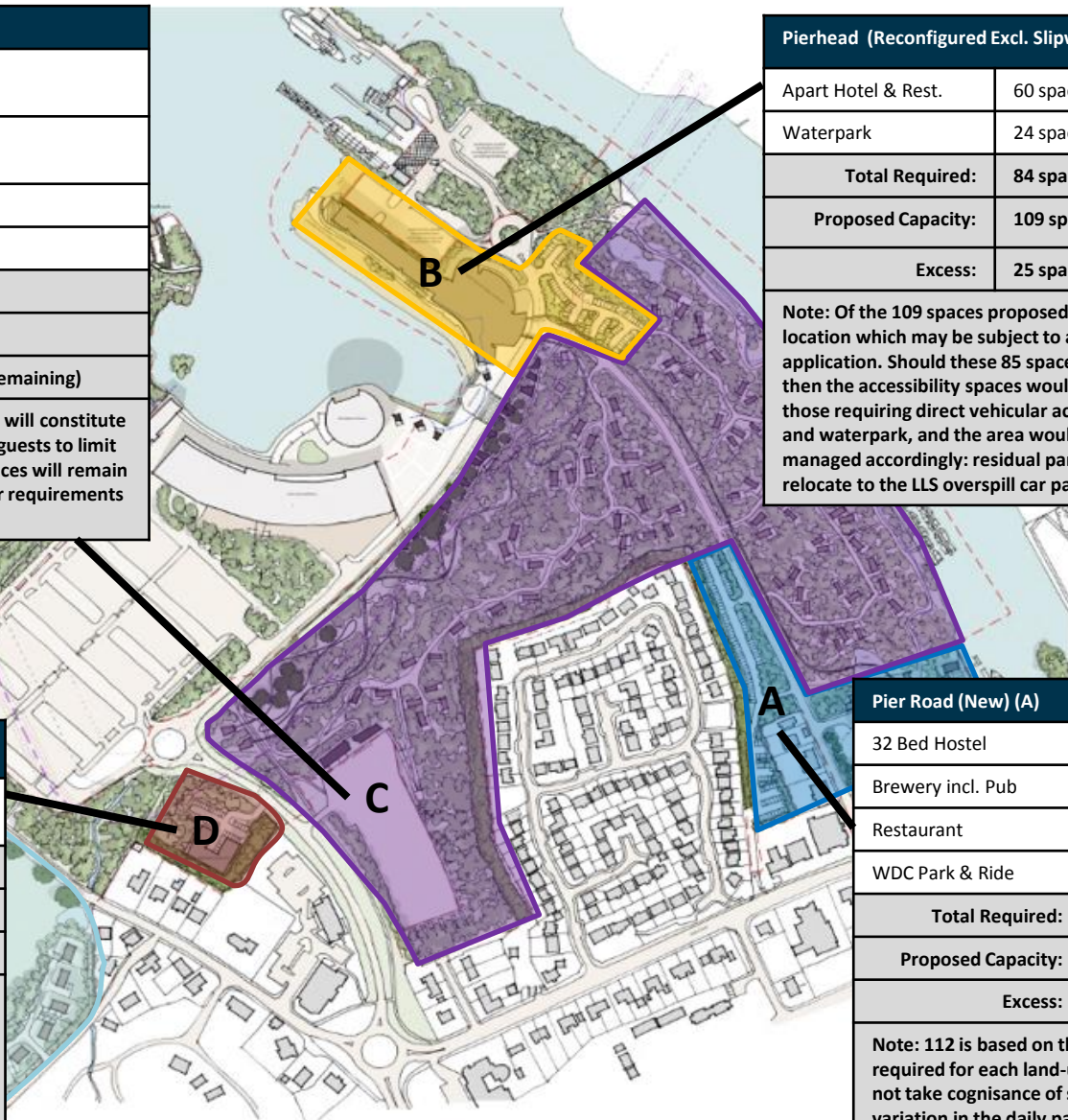
- **Expansion and Integration of Cycling Hire Schemes** – we are in agreement that both internal and strategic trips to the site will be important, and we've noted previously in discussion with ScotRail Abellio how this could, in part, be enhanced through information provided at the point of booking accommodation and/ or travel tickets and incentivising the use of other modes as far as reasonably practicable;
- **Tailored Ticketing Opportunities and Promotional Material** – preliminary discussions have been held with ScotRail Abellio around some of the potential opportunities for ticketing and promotional material and we welcome the further detail provided in this section. Further discussions will be necessary to explore the detail and extent of these, and we'd welcome the opportunity for these to commence early in the process when the proposals advance to the detailed design stage;
- **Future Capacity Demand and Rail Frequencies** – comments are noted;
- **Travel Plan Proposals and Integration** – commentary reiterates the need for dialogue at the relevant stages in the advancement of the development and design proposals and a collaborative approach remains a key ambition for the development team. We look forward to working with ScotRail Abellio at the relevant subsequent stages;
- **Enhancement and Consolidation of Passenger Experience** – we recognise the importance of proactive provision for equality groups and will take cognisance of the commentary at the relevant latter stages of the development and design proposals; and
- **S75 Contributions and Community Involvement** – noted.

LLS Overspill Car Park (C) – 359 Spaces Existing	
Woodland Lodges Riverfront (43)	65 spaces
Woodland Lodges Drumkinnon (32)	48 space
Boathouse Accommodation	1 space
Staff & Servicing (residual)	11 spaces*
<b>Total Required:</b>	<b>125 spaces</b>
<b>Proposed Capacity</b>	<b>125 spaces</b>
<b>Excess:</b>	<b>234 spaces (remaining)</b>
<p>Note: Some of the existing overspill parking area will constitute long-stay overnight parking for accommodation guests to limit the volume of traffic through the site. Ample spaces will remain within the overspill car park for events and other requirements as appropriate to the order of 234 spaces.</p>	

Pierhead (Reconfigured Excl. Slipway Ops) (B)	
Apart Hotel & Rest.	60 spaces
Waterpark	24 spaces
<b>Total Required:</b>	<b>84 spaces</b>
<b>Proposed Capacity:</b>	<b>109 spaces (incl. 24 accessible)</b>
<b>Excess:</b>	<b>25 spaces</b>
<p>Note: Of the 109 spaces proposed, 85 are sited on a location which may be subject to a future planning application. Should these 85 spaces require to relocate, then the accessibility spaces would remain and cater for those requiring direct vehicular access to the apart hotel and waterpark, and the area would be reconfigured/ managed accordingly: residual parking demand would relocate to the LLS overspill car park.</p>	

Staff and Servicing Area (D)	
Staff & Servicing 900sqm	45 spaces
<b>Total Required:</b>	<b>45 spaces</b>
<b>Proposed Capacity:</b>	<b>34</b>
<b>Excess:</b>	<b>-11* spaces</b>
<p>Note: Due to shift patterns it is anticipated that 34 spaces located at the staff and service area will be ample, however, residual demand or ancillary parking could be accommodated in the LLS overspill car park as appropriate. The intention is to encourage employees to walk and cycle to work as far as reasonably practicable.</p>	

Pier Road (New) (A)	
32 Bed Hostel	8 spaces
Brewery incl. Pub	30 spaces
Restaurant	30 spaces
WDC Park & Ride	44 spaces
<b>Total Required:</b>	<b>112 spaces</b>
<b>Proposed Capacity:</b>	<b>109 spaces</b>
<b>Excess:</b>	<b>-3</b>
<p>Note: 112 is based on the number of spaces required for each land-use in isolation and does not take cognisance of shared uses of spaces nor variation in the daily parking demand profile. The proposed capacity of 109 spaces is considered ample across the development uses.</p>	



Woodbank Site (E)	
6 Private Houses (2 beds)	12 spaces
15 Private Flats (7 x 1 bed, 8 x 2 bed)	23 spaces
19 Woodland Lodges	38 spaces
16 Woodland Lodges	32 spaces
15 Woodland Bothies	23 spaces
6 Holiday Apartments	11 spaces
<b>Total Required:</b>	<b>139 spaces</b>
<b>Proposed Capacity:</b>	<b>139 spaces</b>
<b>Excess:</b>	<b>0 spaces</b>
<b>Note: As a new site, parking provision will be provided in compliance with relevant parking standards and can be accommodated within the site extents.</b>	



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